



## THE KANSAS CITY SOUTHERN RAILWAY COMPANY

427 West 12<sup>th</sup> Street  
Kansas City, Missouri 64105-1403



### UTILITY PERMIT PROCESS INSTRUCTIONS

#### How to apply for a permit:

In an effort to effectively facilitate the permit process, The Kansas City Southern Railway Company ("KCS") has partnered with Jones Lang LaSalle Americas, Inc. ("JLL") who is committed to provide professional Real Estate Services. This website is intended to provide you with all the information you should need to apply for a permit to access KCS's property through JLL.

Complete the application, print and mail with the following:

- The **\$2,000.00** (non-refundable) **processing fee**, check made payable to Jones Lang LaSalle Americas, Inc. KCS requires the processing fee be sent with the application, or it will be *returned*.
- Insurance certificate evidencing below coverages and naming The Kansas City Southern Railway Company as additional insured.
- **Four (4) sets of drawings** of the area to be occupied pursuant to Section G of the Specific Requirements (pdf documents via email is preferred)

**TO: Jones Lang LaSalle Americas, Inc.**  
**Attn: Sylvia Schmidt - Permits Department**  
**3017 Lou Menk Drive, Suite 100**  
**Fort Worth, Texas 76131-2800**  
**Phone: (817) 230-2688**  
**Fax: (817) 306-5521**  
**Email: sylvia.schmidt@am.jll.com**

If the information is not complete or is incorrect, it will delay the permit process.

#### Permit Process:

1. Once received by JLL, the application and drawing will be forwarded to the engineering firm to perform the engineering specification review and prepare the Exhibit "A" drawings for the contract and obtain KCS approval. Information provided by the applicant will be used to prepare the Exhibit "A". Therefore, please ensure that all information is accurate. **This process takes approximately six (6) weeks.** An additional processing fee up to \$1,200.00 may be charged for each modification to Exhibit "A". However, the additional fee could exceed \$1,200.00 in circumstances where the change requires additional engineering or signaling investigation beyond those considered standard.
2. When the Exhibit "A" is completed, provided KCS approves the application, a Contract will be prepared and three (3) copies will be forwarded to you for original signature. If the application is not approved, the applicant will be notified in writing with reasons for disapproval.
3. Once received by your office, forward the signed Contracts (3 copies with original signatures), along with the appropriate **contract payment** to JLL.
4. Pursuant to Section 14 of the application, the applicant must contact the engineering firm, Bartlett & West, thirty (30) days in advance of construction to contract for construction observation and flagging. Please note that the permit contract will not be executed by KCS or forwarded to the applicant until evidence has been provided that the applicant has contracted with the engineering firm for construction observation and flagging.
5. The final contracts with original signatures are presented to KCS for execution provided fees have been received, construction observation and flagging have been contracted for, and insurance had been approved.
6. Once the contract is executed, one (1) original will be returned for your files.



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**Process Time:**

Please be advised that the average time period for completion of this process is 6 to 8 weeks from the time that the application is received. Every effort will be made to complete this process in a timely manner.

Installation of utility will not begin until all documents (signed contract, insurance and fees) have been received and contract has been fully executed by The Kansas City Southern Railway Company.

**Insurance Requirements for the following Agreements:**

Pipeline License, License for Overhead or Underground Wire line

- **Comprehensive General Liability & Contractual Liability Insurance.** \$2,000,000 per occurrence and an aggregate of \$4,000,000 minimum limits.
- **Automobile Liability Insurance.** \$1,000,000 combined single limit.
- **Workers Compensation Insurance.** Minimum statutory worker's compensation coverage for all covered employees who are on Licensee's property.
- **Railroad Protective Liability Insurance.** Naming The Kansas City Southern Railway Company as insured. \$2,000,000 per occurrence and \$6,000,000 aggregate. For information on railroad protective liability insurance, please visit <http://www.kcsouthern.com/en-us/GeneralPublic/Pages/RRProtectiveLiabilityProgram.aspx> or [www.railroadprotectivesolutions.com](http://www.railroadprotectivesolutions.com), or email questions to [rpl@railroadprotectivesolutions.com](mailto:rpl@railroadprotectivesolutions.com).
- No policy will be canceled or materially altered without first giving KCS thirty (30) day's written notice.
- Commercial General Liability naming The Kansas City Southern Railway Company as additional insured.
- Waiver of subrogation in favor of KCS.
- All policies will be primary to any insurance or self-insurance KCS may maintain for acts or omissions of Licensee or anyone for whom Licensee is responsible.

**Please note:** These limits are subject to change at any time without notice. An Agreement will be provided to you, which contains details concerning insurance requirements.



**APPLICATION FOR**

License to Install Pipeline or Wire Line Encroaching Over, Under or Parallel to the Railroad Tracks

**SUBMIT WITH NON-REFUNDABLE \$2,000.00 PROCESSING FEE PAYABLE TO**

Jones Lang LaSalle Americas, Inc.  
 Attn.: Sylvia Schmidt  
 3017 Lou Menk Drive, Suite 100  
 Fort Worth, Texas 76131-2800  
 Phone: 817-230-2688  
 Email: sylvia.schmidt@am.jll.com

1. **Date:** \_\_\_\_\_

**2. Applicant Information**

Company Name: \_\_\_\_\_

Attn.: \_\_\_\_\_

Street Address of Applicant: \_\_\_\_\_

City, State, Zip Code: \_\_\_\_\_

Check One:     Individual                       Company                       Partnership  
                    Corporation                       Municipality                       Other (Specify) \_\_\_\_\_

Contact Person (Preferably Engineer): \_\_\_\_\_ Phone #: \_\_\_\_\_

Fax #: \_\_\_\_\_ Cell #: \_\_\_\_\_ Email: \_\_\_\_\_

**3. Type of Encroachment (Crossing)**

Check All That Apply:     Overhead     Underground     Parallel

**If Overhead**, vertical clearance is \_\_\_\_\_ (in feet and inches) a 28 foot (28') minimum is required from top of rail to bottom of overhead crossing point (maximum sag at 212°).

**If Underground Crossing**, depth of casing/carrier pipe: \_\_\_\_\_ feet below base of rail and \_\_\_\_\_ feet below ground surface or bottom of ditches, whichever is deeper (if (a) casing is used: minimum ten feet (10') below base of rail and minimum six feet (6') from natural ground surface or from bottom of ditches; (b) if casing is not used: minimum ten feet (10') below base of rail and minimum ten feet (10') from natural ground surface or from bottom of ditches).

**If Underground Parallel**, distance \_\_\_\_\_ measured at right angle from centerline of tracks to encroachment (parallel and minimum twenty-five feet (25') feet away, then minimum 72 inches (72") ground cover; parallel and greater than twenty-five feet (25') away, then minimum ground cover of sixty inches (60")). If the request is within fifty feet (50') of KCS right-of-way, utility line shall be at least ten feet (10') below the ground for the entire KCS right-of-way.

*If an existing crossing is to be abandoned, please see **Section D – Abandonment** on the Specific Requirements sheet.*

**4. Location** (please provide all the information you can and identify two (2) markers of location information that can independently allow for verification of the wire line location in Section 4 and Section 5).

If crossing, relation to centerline of Railroad Tracks at Railroad Mile Post \_\_\_\_\_ plus \_\_\_\_\_ feet.

If parallel, beginning Railroad Mile Post \_\_\_\_\_ plus \_\_\_\_\_ feet ending Railroad

Mile Post \_\_\_\_\_ plus \_\_\_\_\_ feet.

If spur, distance from point of switch \_\_\_\_\_ feet.

In or near \_\_\_\_\_ (name of City, Town or Village), \_\_\_\_\_ (State)

in \_\_\_\_\_ (Parish/County).

Section \_\_\_\_\_ Township \_\_\_\_\_ Range \_\_\_\_\_

**5. Other Distances** (please provide all the information that you can)

Distance from inside face of back wall on nearest Railroad Bridge: \_\_\_\_\_ Bridge #: \_\_\_\_\_

Distance from centerline of DOT-AAR #: \_\_\_\_\_ Name: \_\_\_\_\_ DOT #: \_\_\_\_\_

**6. Will installation be in a dedicated street or highway?**  Yes  No

*If yes, please provide documented information proving ownership of street or highway right-of-way.*

**7. Angle of Crossing**

\_\_\_\_\_ ° - \_\_\_\_\_ ' - \_\_\_\_\_ " (if crossing track, not less than 45 degrees).

**8. Wire Line Commodity**

Telephone  Power Line (Voltage \_\_\_\_\_)

Fiber Optic - **Number of Innerducts** \_\_\_\_\_  Other (Please Specify \_\_\_\_\_)

*Please see **Section A – General** on the Specific Requirements sheet for standards used.*

***If encroachment is an overhead wire line, please proceed to the end of this Application and provide signature.***



**12. Installation Method**

Method of installing pipe \_\_\_\_\_

*Dry Bore and Jack, Conventional Dry Bore or Directional Drill up to ten inches (10"). Wet boring **WILL NOT** be permitted.*

*Please see **Section C – Boring** and **Section F – Shoring** on the Specific Requirements sheet for additional requirements.*

**13. Venting**

Will casing pipe be vented?  Yes  No Diameter: \_\_\_\_\_ (minimum two (2) inches)

Number of Vents: \_\_\_\_\_ Vent Height above ground surface: \_\_\_\_\_ (minimum four (4) feet)

*Pipes carrying flammable contents must be vented. Vents are to be located at ends of casing off of railway property and to be shown on drawing.*

**14. Additional Fees**

All excavations and installations on Railway’s Right of Way will require construction observation, documentation, monitoring and flagging by Railway or its authorized representative. Applicant or its contractor must contact Bartlett & West, the Railway’s Authorized Representative, at least thirty (30) days in advance of the scheduled construction date. Applicant will contract directly with Bartlett & West for the above services. The contact at Bartlett & West is Marc Villines at (785) 272-2252, Ext. 110, or by email at [KCSrequests@bartwest.com](mailto:KCSrequests@bartwest.com). All costs associated with construction observation, monitoring and flagging are the sole responsibility of the applicant. Construction Observation fees are approximately \$1,500 per day. **No permit agreement for crossing or encroachment will be executed by Railway until applicant has contracted with Bartlett & West for the construction observation, monitoring and flagging.**

**Each encroachment will also be subject to an annual or one-time fee, dependent on the type of installation and length of encroachment.** If you will need to know these amounts before application, please contact Sylvia Schmidt at [sylvia.schmidt@am.jll.com](mailto:sylvia.schmidt@am.jll.com) or 817-230-2688.

**15. Existing Agreement?**  Yes  No

If replacing or repairing an existing line, a copy of the current permit agreement with Railway **must** be provided and accompany this application.

**16. Was this service requested by KCS?**  Yes  No

If yes, who requested and phone number? \_\_\_\_\_ Phone #: \_\_\_\_\_

Is this installation in conjunction with a track or track expansion project?  Yes  No

If yes, KCS contact name and phone number: \_\_\_\_\_ Phone#: \_\_\_\_\_

By signing below, **applicant agrees to pay all fees associated with the installation of this utility.**

\_\_\_\_\_  
Name of Applicant

\_\_\_\_\_  
Signature of Applicant

\_\_\_\_\_  
Date



## SPECIFIC REQUIREMENTS FOR

### *License to Install Pipeline or Wire Line Encroaching Over, Under or Parallel to the Railroad Tracks*

#### A. GENERAL

Pipelines will be reviewed for compliance with The American Railway Engineering and Maintenance of Way Association (AREMA) standards. AREMA standards are available for purchase online at [www.arema.org](http://www.arema.org) and specific parts may be purchased. The following Parts are utilized for underground installations:

- 5.1 Specifications for Pipelines Conveying Flammable Substances
- 5.2 Specifications for Uncased Pipelines within the Railway Right-of-Way
- 5.3 Specifications for Pipelines Conveying Non-Flammable Substances
- 5.4 Specifications for Overhead Pipelines Crossings
- 5.5 Specifications for Fiber optic "Route" Construction on Railroad Right-of-Way

Wire lines will be reviewed for compliance with the 2002 National Electric Safety Code Article 232, Table 232.1.

#### B. CASING

It is preferred that all pipelines have steel carriers and are encased in steel pipes. Uncased carrier pipe transporting or conveying natural gas must be steel and must be at a minimum depth of ten feet (10') from base of rail to top of carrier pipe, and a minimum of ten feet (10') between top of carrier pipe and natural ground or ditch bottom on Railway Right-of-Way. PVC, HDPE, and ductile iron carrier pipes must be encased in steel casing pipes extending from right-of-way line to right-of-way line.

Inside diameter of casing pipe shall be at least two inches (2") greater than the largest outside diameter of the carrier pipe, joints, or couplings, for carrier pipes less than six inches (6") in diameter; and at least four inches (4") greater for carrier pipe six inches (6") and over in diameter.

Additionally, KCS requires that all pipelines carrying liquid flammables must be encased across the entire width of the Railway Right-of-Way.

#### C. BORING

Bore pits **MUST** be placed outside of the right-of-way of the railway. If there are extenuating circumstances which require a bore pit to be placed within the limits of the right-of-way, specific application must be made outlining the need for encroachment upon the railway right-of-way and written permission must be obtained before construction is allowed.

Misaligned bores will not be pulled and re-drilled, but abandoned in place. Please see Section D – Abandonment for requirements. New bore will be moved at least five feet (5') to either side of the misaligned attempt and re-drilled.

**NO WET BORES WILL BE ALLOWED.** Directional boring must utilize a machine which retrieves a majority (95%) of the drilling fluid slurry used in the placement of lines/casings. Under paragraph 5.5.3.4.8 of AREMA Standards, the maximum size of the finished hole is ten inches (10") for fiber optic installation. AREMA specifically states in paragraph 5.5.3.4.6 that wet bores are not allowed for installations in railroads' right-of-way. Wet bores refers to the use of liquids to displace soil.



## SPECIFIC REQUIREMENTS FOR

### *License to Install Pipeline or Wire Line Encroaching Over, Under or Parallel to the Railroad Tracks* *Continued*

#### D. ABANDONMENT

Any lines being abandoned must be filled with a suitable flow able fill material and sealed.

When carrier pipe is centered in casing, provide end seals as manufactured by Link-Seal, Pipeline Seal and Insulator, Inc. (PSI), Raychem, or approved equal, as required for the application. Seal material shall be DFO and oil resistant. Provide coated steel or non-metallic casing spacers as manufactured by PSI or approved equal.

When carrier pipe is not centered in casing, provide end seals as manufactured by Pipeline Seal and Insulator, Inc. (PSI), Raychem, or approved equal, as required for the application. Seal material shall be DFO and oil resistant. Provide coated steel or non-metallic casing spacers as manufactured by PSI or approved equal.

#### E. SIGNAGE

All underground installations will be marked at entry and exit to railway right-of-way with a sign mounted on a substantial post at least four feet (4') above ground surface. Parallel encroachments will be marked with a sign every five hundred feet (500') and at every road crossing with a sign mounted on a substantial post at least four feet (4') above ground surface. The sign must convey the commodity conveyed, name of licensee, address of licensee, emergency phone number and any emergency response information.

#### F. SHORING

All underground installations will be subject to an additional shoring review. All bore pits will be subject to OSHA requirements and may also be subject to AREMA standards (Part 28 – Temporary Structures for Construction). Using the print titled “General Shoring Requirements” Date: 04/21/06, Rev.: 04/27/07; a determination will be made on the need for a shoring review. If this additional review is required, please add two (2) weeks to the estimated time to process your application. Additional documentation including shoring calculations and design details signed and sealed by a structural engineer, will need to be submitted to KCS Engineering department for review. The engineering review cost will need to be paid in advance at the rate of \$3,000.00 per installation. This amount is **IN ADDITION** to the standard \$2,000.00 processing fee.

#### G. APPLICATION SUBMITTAL

Applicant to furnish one (1) copy of the fully completed Application and four (4) 8-1/2" x 11" or 8-1/2" x 14" prints showing drawing with crossing measured from the nearest Mile Post, Bridge back wall, point of mainline switch or DOT-AAR numbered vehicular crossing, as well as limits of railroad (and street or highway, if involved) right-of-way; plus four (4) prints of profile sketch (cross section) of actual situation showing relationship of tracks, contours of ground and surface ditches, the encroaching line, other buried or wire lines (if known), etc. This is to be used to prepare exhibit for agreement. **PRINT MUST SHOW NORTH.**

#### H. SCHEDULE

The standard review takes approximately six (6) weeks. These are estimated times for review and are not guaranteed. When application has been processed and Agreement executed, allow thirty (30) days for scheduling of applicable flagging and construction observation services.



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### DIRECTIONAL DRILLING SPECIFICATIONS

1. Prior to drilling, the contractor is to provide the following information to KCS or the designated KCS representative for review and approval before any construction activities commence:
  - Type of drilling fluid and additives to be used (straight water is not acceptable);
  - Anticipated drilling fluid volume used for initial bore and pull back of casing pipe;
  - Anticipated drilling fluid pressure;
  - Anticipated drilling rate;
  - Anticipated drilling fluid volume recovery (minimum 95%).
2. Fluid volume, fluid pressure, and pumping rate are to be monitored by the drilling contractor (calibrated fluid tanks are to be used) and are to be provided to the KCS inspector.
3. The bore is to be of constant grade (under the KCS Right-of-Way). No sag in pipe grade is allowed.
4. The back reamer is to have a maximum diameter of ten inches (10"). Maximum casing pipe diameter is eight inches (8").
5. The contractor is to track the drill head during drilling and record depth of cover below existing surface and provide this information to the KCS inspector.
6. If the casing becomes stuck, the end is to be cut off, the casing filled with flow able fill, and abandoned. An exhibit indicating the horizontal and vertical location of the abandoned casing must be provided to KCS. Casing pipe is only to be pulled into place with the drilling equipment. The use of a dozer, backhoe, etc. to pull casing pipe into place is not permitted.
7. If the fluid volume significantly exceeds the expected volume, or if the fluid pressure drops significantly, drilling is to stop, and the bore is to be filled with bentonite slurry and abandoned.
8. Casing ends must be sealed, with the contractor informing KCS of the type of seal they propose to use.
9. Identifying marker signs indicating the presence of the new utility must be posted within 24 hours of casing and carrier pipe material installation and backfilling of any pits and dressing of railroad right-of-way. The drilling contractor will be assessed an inspection fee of \$1,500.00/day until the signs are placed. The Inspector will stay on site and a fee of \$1,500.00 per day will be assessed until the signs are in place.
10. An exhibit indicating the plan view, skew, and vertical elevations of the casing must be submitted to KCS upon completion of the project.
11. Initial bore pit and receiving pit are to be located off of railroad right-of-way. If it is necessary for the pit(s) to be on railroad right-of-way, shoring plans for railroad loading must be provided to KCS for review and approval prior to the beginning of work.

**TRACK PROTECTION SHORING:**

ALL DIMENSIONS ARE MEASURED PERPENDICULAR TO C OF TRACK.

PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR SHALL SUBMIT FOR APPROVAL BY THE RAILROAD ENGINEER, DETAILED PLANS INDICATING THE NATURE AND EXTENT OF THE TRACK PROTECTION SHORING PROPOSED. THE CONTRACTOR SHALL PROVIDE AND INSTALL TRACK PROTECTION SHORING BEFORE COMMENCING EXCAVATION.

SHORING WITHIN ZONE A SHALL BE DESIGNED FOR COOPER E80 LIVE LOAD SURCHARGE, IN ADDITION TO ALL OTHER APPLICABLE LOADS. THE RAILROAD MAY IMPOSE MORE STRINGENT REQUIREMENTS AS CONDITIONS WARRANT.

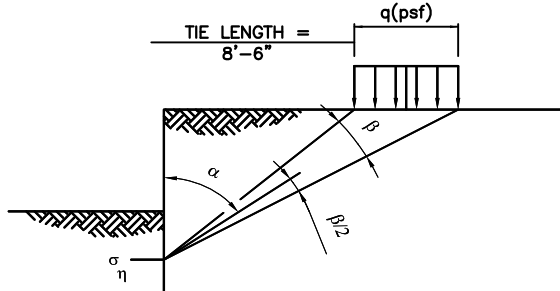
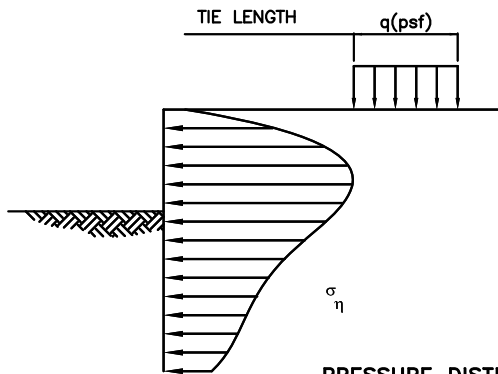
FOR EXCAVATIONS WHICH ENCROACH INTO RAILROAD LIVE LOAD SURCHARGE ZONE A, SHORING PLANS WILL BE ACCOMPANIED BY A COPY OF DESIGN CALCULATIONS, AND BOTH MUST BE STAMPED BY A REGISTERED PROFESSIONAL ENGINEER IN THE APPLICABLE STATE.

THE PRESSURE AT A GIVEN POINT OF A CONTINUOUS STRIP OF SURCHARGE LOAD q(psf) PARALLEL TO SHORING SHALL BE COMPUTED BY:

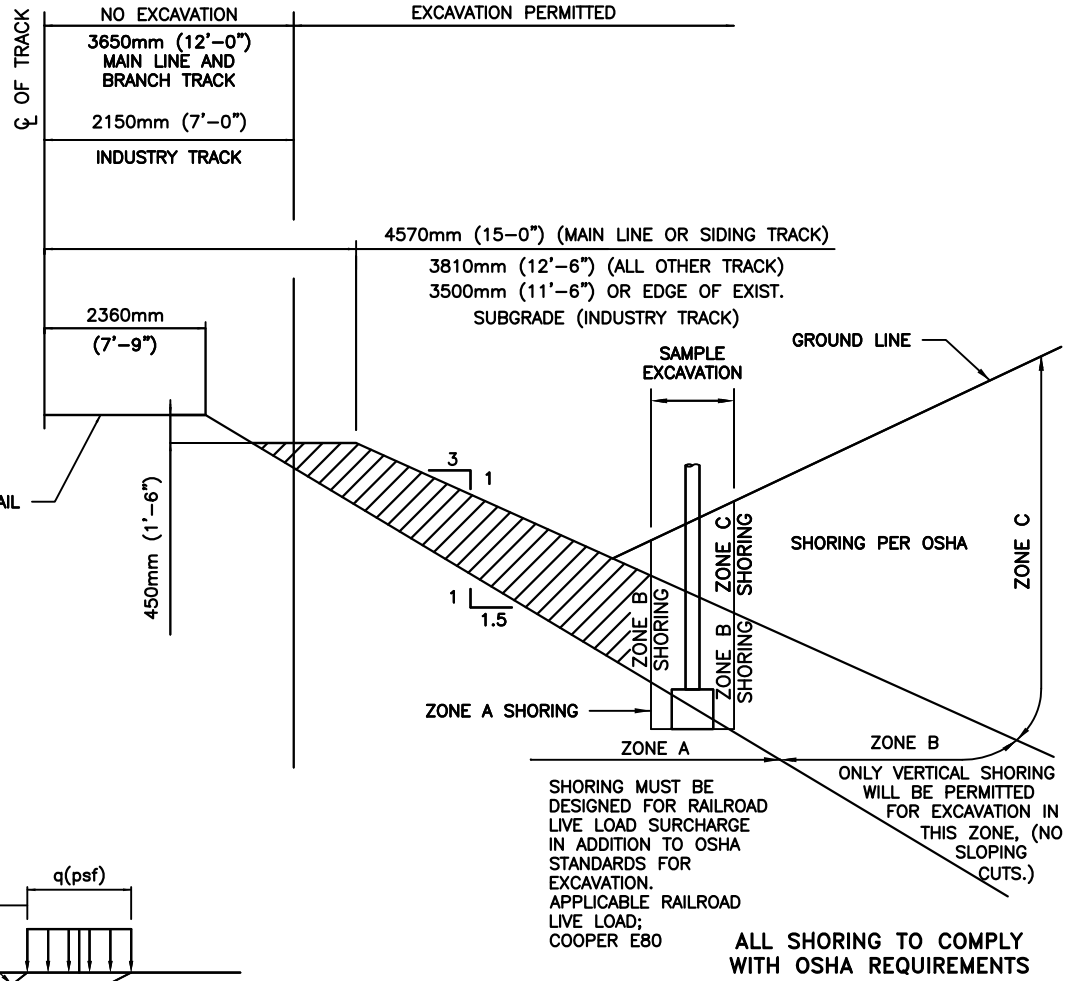
$$\sigma_{\eta} = \frac{2q}{\pi} [\beta - \sin \beta \cos 2\alpha]$$

WHERE ANGLES  $\alpha$  AND  $\beta$  ARE EXPRESSED IN RADIAN.

$$q = \frac{80,000 \text{ lbs}}{(5 \text{ FEET})(8.5 \text{ FEET})}$$



**PRESSURE DISTRIBUTION FOR STRIP LOAD (AREMA FIGURE 8.20-2)**



SHORING MUST BE DESIGNED FOR RAILROAD LIVE LOAD SURCHARGE IN ADDITION TO OSHA STANDARDS FOR EXCAVATION. APPLICABLE RAILROAD LIVE LOAD; COOPER E80

ONLY VERTICAL SHORING WILL BE PERMITTED FOR EXCAVATION IN THIS ZONE, (NO SLOPING CUTS.)

**ALL SHORING TO COMPLY WITH OSHA REQUIREMENTS**



**K.C.S. STANDARDS**

DATE: 4-21-06  
 REV: 4-27-07  
 SCALE: NONE  
 DRAWN BY: RED

APPROVED BY:  
 \_\_\_\_\_  
 CHIEF ENGINEER

**GENERAL SHORING REQUIREMENTS**

DRAWING NO.  
**BR-122**